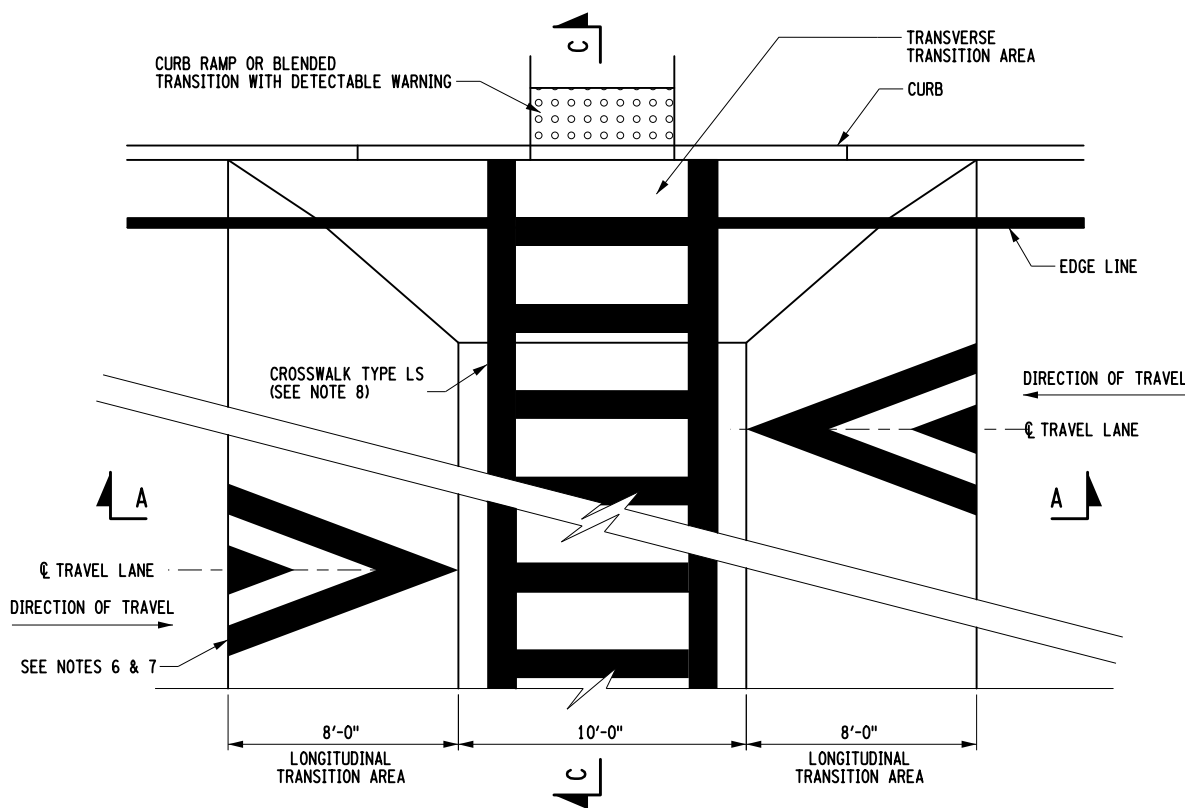
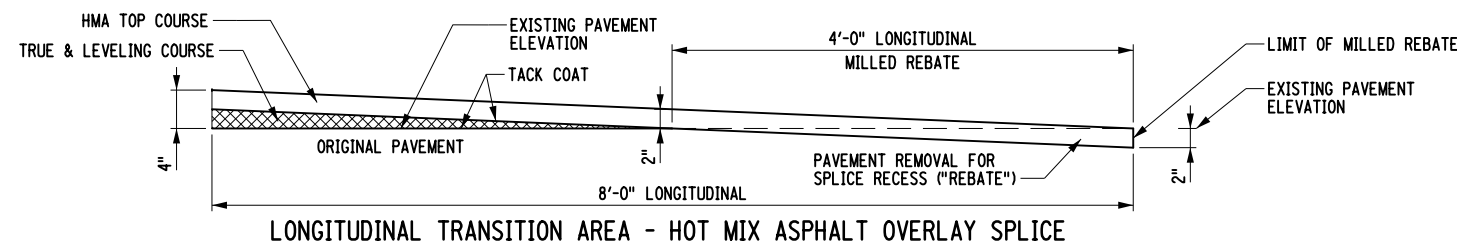


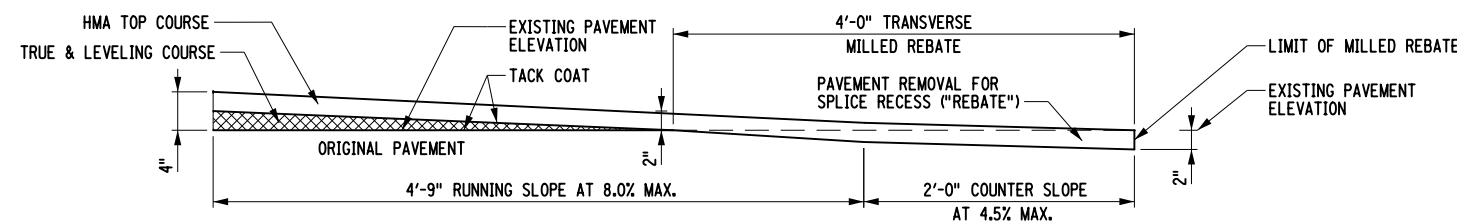
PLAN VIEW WITHOUT OPTIONAL DRAINAGE TREATMENT



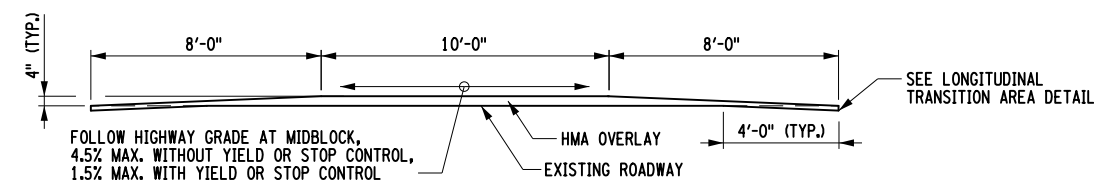
PLAN VIEW WITH OPTIONAL DRAINAGE TREATMENT  
(SEE NOTE 2)



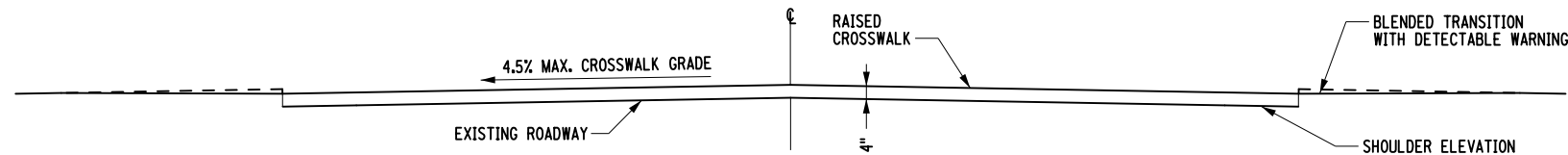
LONGITUDINAL TRANSITION AREA - HOT MIX ASPHALT OVERLAY SPLICE



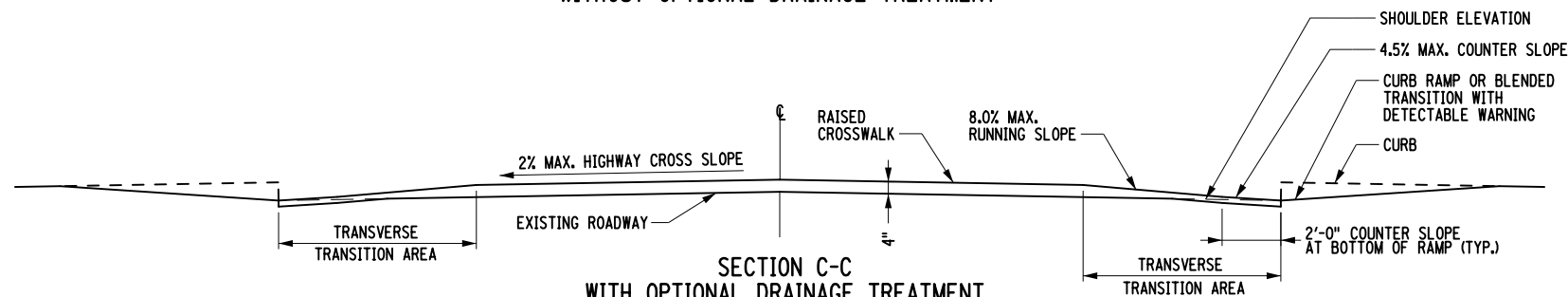
TRANSVERSE TRANSITION AREA - HOT MIX ASPHALT OVERLAY SPLICE  
(SEE NOTE 2)



SECTION A-A  
WITH OR WITHOUT OPTIONAL DRAINAGE TREATMENT



SECTION B-B  
WITHOUT OPTIONAL DRAINAGE TREATMENT



SECTION C-C  
WITH OPTIONAL DRAINAGE TREATMENT  
(SEE NOTE 2)

**NOTES:**

1. RAISED CROSSWALK REQUIRES STORM WATER TO BE COLLECTED AND CONVEYED TO AN APPROPRIATE LOCATION.
2. THE OPTIONAL DRAINAGE TREATMENT SHALL ONLY BE USED IN RETROFIT SITUATIONS. THE TRANSVERSE TRANSITION AREA DETAIL ABOVE IS FOR USE ON HIGHWAYS WITH CROSS SLOPES. RETROFIT RAISED CROSSWALKS ON ROADWAYS THAT EXCEEDED THIS LIMIT SHALL BE DETAILED IN THE PLANS.
3. THE TRANSITION AREA MAY BE CONCRETE OR HMA, AND WILL BE AS SHOWN ON THE PLANS.
4. ALL SURFACES OF THE MILLED TRANSITION AREA SHALL BE CLEANED. TACK-COAT SHALL BE APPLIED PRIOR TO HMA PLACEMENT. THE COST OF MILLING REBATES AND TACK COAT IN THE HMA OVERLAY SPLICE TRANSITION AREA SHALL BE PAID FOR UNDER THEIR RESPECTIVE ITEMS. THE COST OF LEAVING A NEAT EDGE SHALL BE INCLUDED IN THE MILLING ITEM.
5. SEE SHEET 2 OF 3 FOR PCC RAISED CROSSWALK DETAILS.
6. SEE SHEET 3 OF 3 FOR SIGNING AND PAVEMENT MARKING DETAILS.
7. SIGNING AND PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE MUTCD.
8. FOR CONTRAST OR AESTHETICS, A TYPE S CROSSWALK PAVEMENT MARKING MAY BE USED WITH A COLORED OR TEXTURED CROSSWALK SURFACE.



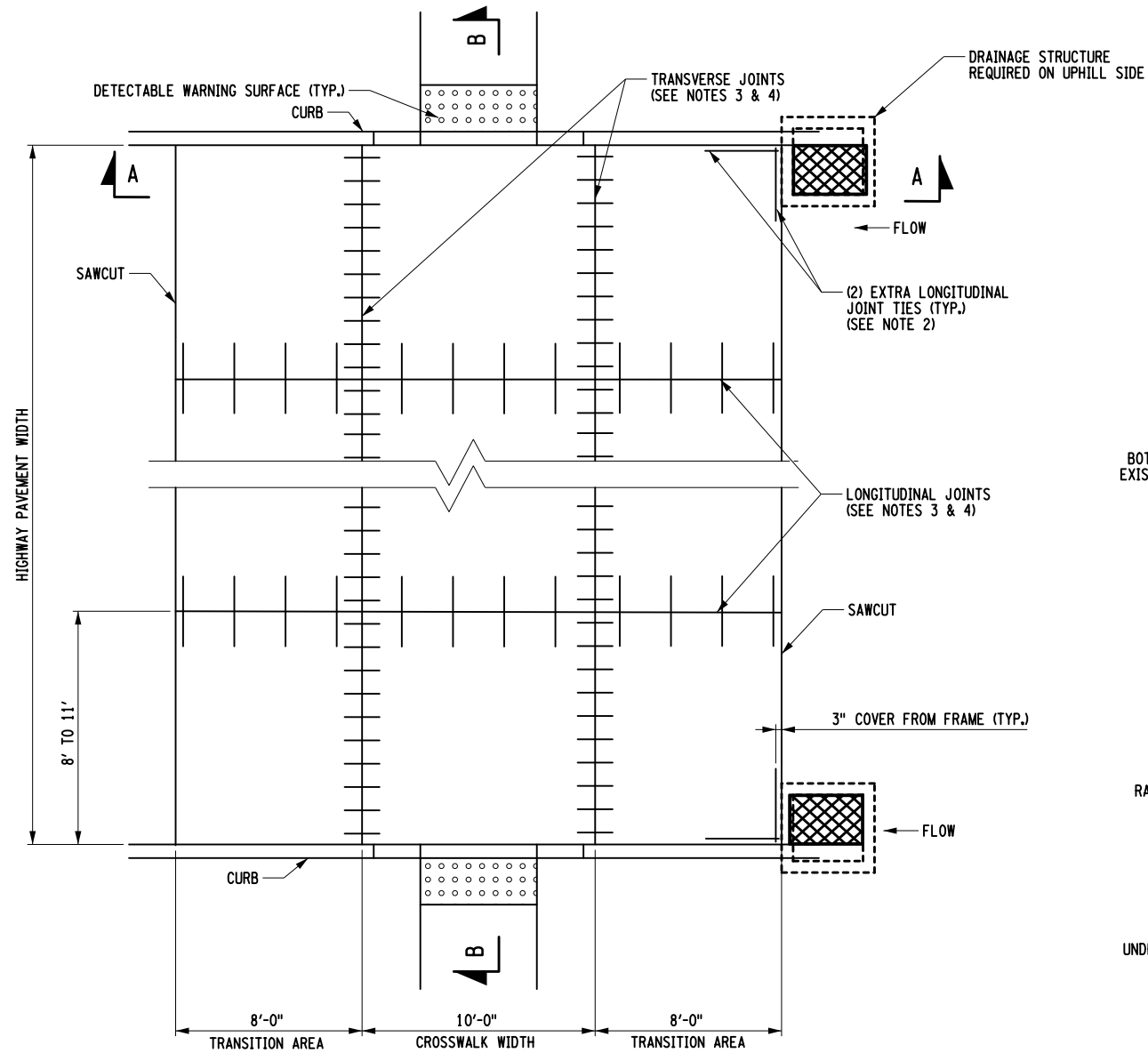
U.S. CUSTOMARY STANDARD SHEET

RAISED CROSSWALK DETAILS  
(SHEET 1 OF 3)

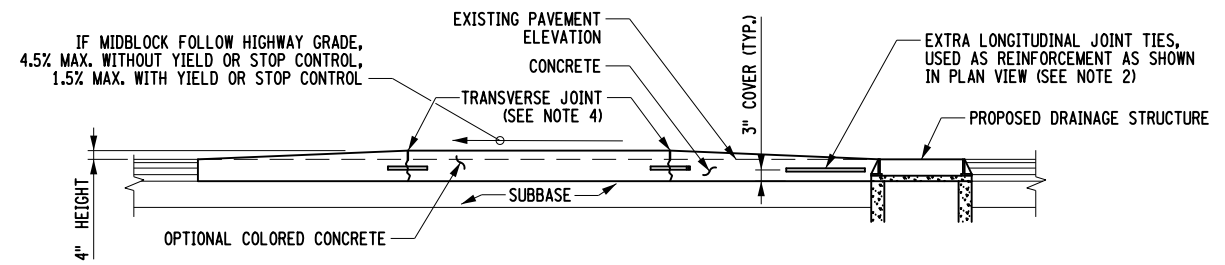
APPROVED MARCH 07, 2016  
/S/ RICHARD W. LEE, P.E.  
DEPUTY CHIEF ENGINEER  
(DESIGN)

ISSUED UNDER EB 16-012

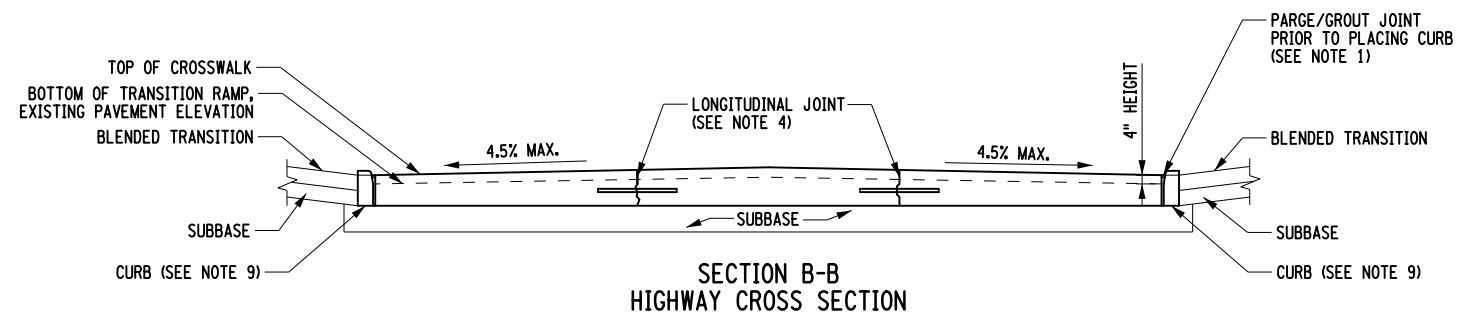
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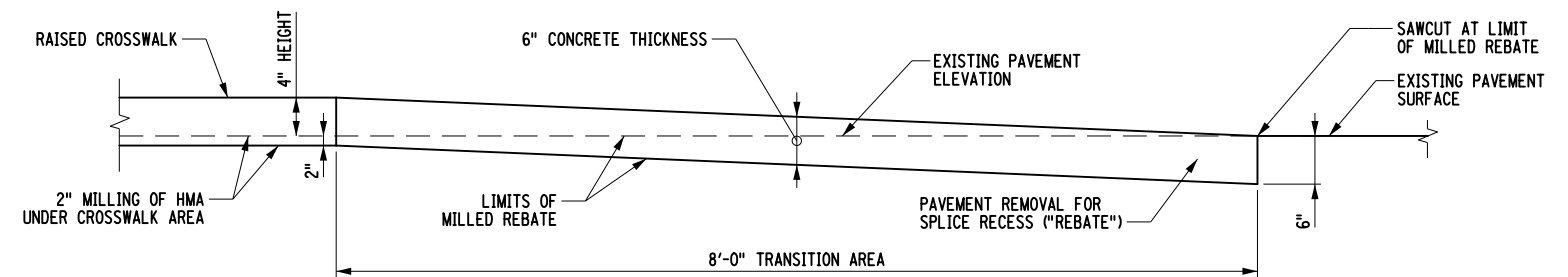
CAST IN PLACE CONCRETE  
RAISED CROSSWALK REINFORCEMENT PLAN  
(SEE NOTE 9)



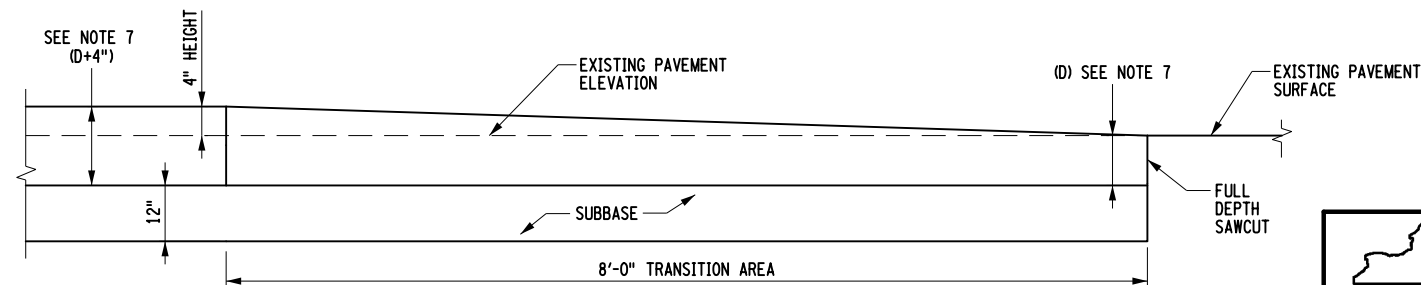
SECTION A-A  
HIGHWAY PROFILE



SECTION B-B  
HIGHWAY CROSS SECTION



OPTIONAL TRANSITION AREA  
FOR PCC OVERLAY SPLICE ON FULL DEPTH HMA  
(SEE NOTE 6)

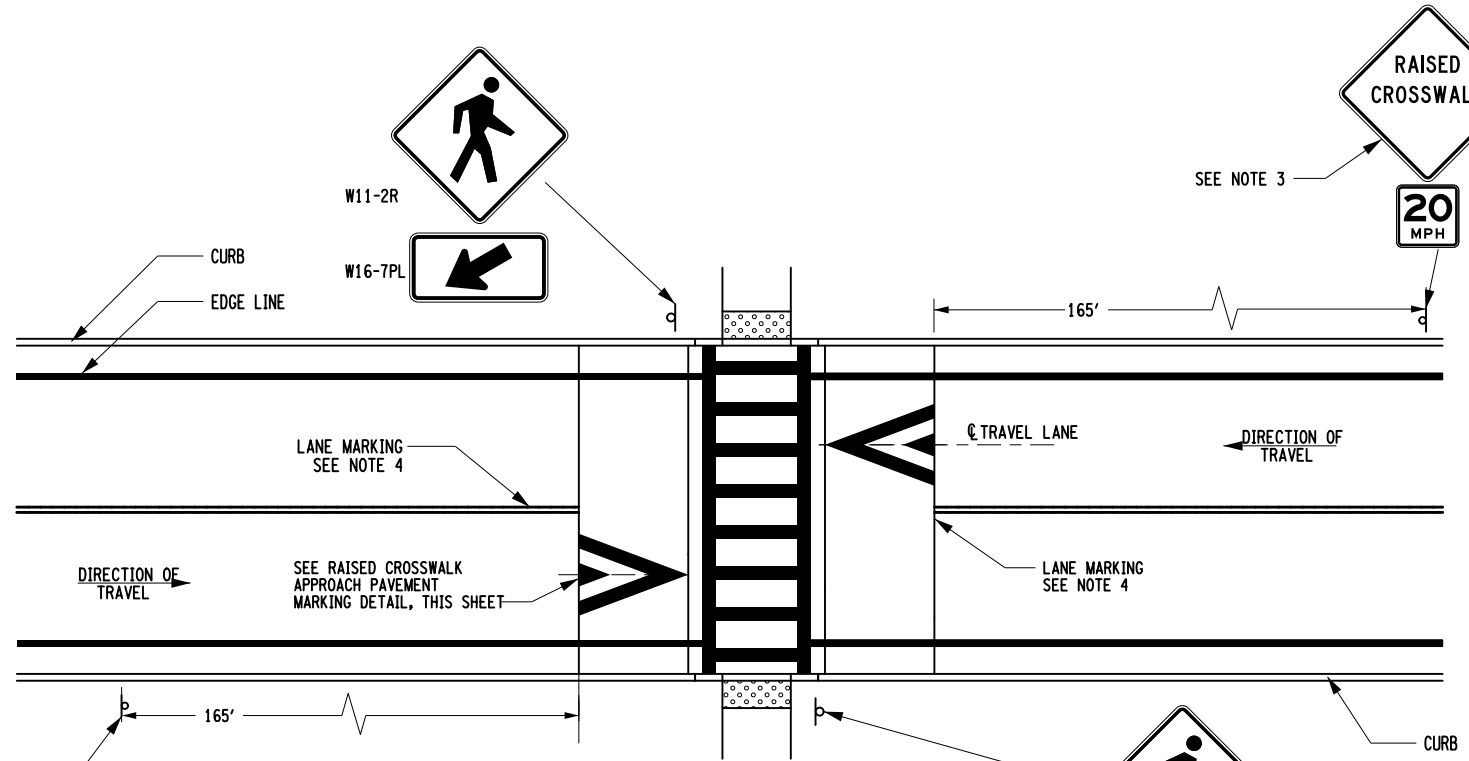


TRANSITION AREA - FULL DEPTH  
(SEE NOTE 8)

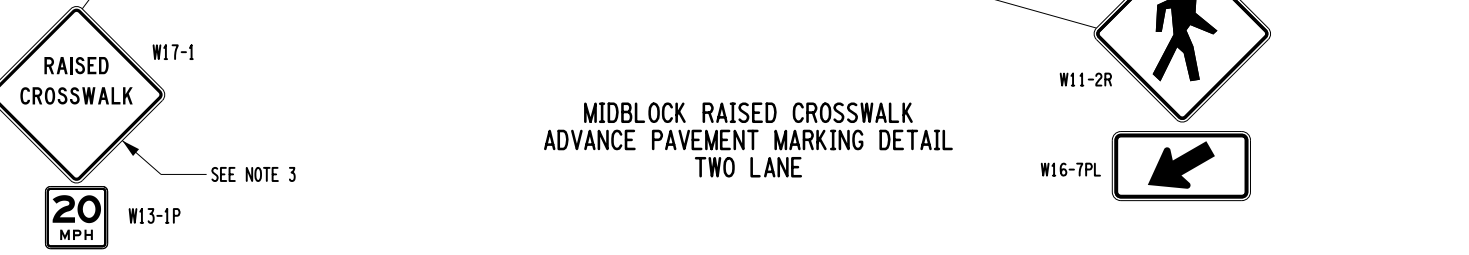
NOTES:

1. GROUT BETWEEN GRANITE/PRECAST CURBS AND CONCRETE PAVEMENT SHALL MEET STANDARD SPECIFICATION 705.21 FOR TYPE S MASONRY MORTAR. CAULKING SHALL CONFORM TO REQUIREMENTS OF CAULKING COMPOUND FOR DRAINAGE STRUCTURES.
2. EXTRA LONGITUDINAL JOINT TIES, USED AS REINFORCEMENT, ARE NEEDED ONLY WHEN DRAINAGE STRUCTURES ARE PRESENT.
3. SEE NYSDOT STANDARD SHEET 502-02 FOR PROPER DOWEL BAR/TIE BAR SPACING.
4. SEE NYSDOT STANDARD SHEETS 502-03 THROUGH 502-07 FOR LONGITUDINAL AND TRANSVERSE JOINT DETAILS.
5. SIGNAGE AND PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE MUTCD. SEE STANDARD SHEET 608-07 SHEET 3 OF 3 FOR SIGNING AND PAVEMENT MARKING DETAILS.
6. MAY BE USED FOR HIGHWAYS WITH  $\leq 20,000$  ADT,  $\leq 5\%$  TRUCKS, AND A PAVEMENT SURFACE SCORE OF 7 OR GREATER.
7. DETERMINE PCC THICKNESS (D) FROM THE CONTRACT DOCUMENTS.
8. FOR PCC PAVEMENT, COMPOSITE PAVEMENTS, AND HMA PAVEMENTS NOT MEETING NOTE 6, FULL DEPTH REPLACEMENT IS REQUIRED.
9. SEE CURB & GUTTER STANDARD SHEETS FOR ANCHOR REQUIREMENTS, IF APPLICABLE.

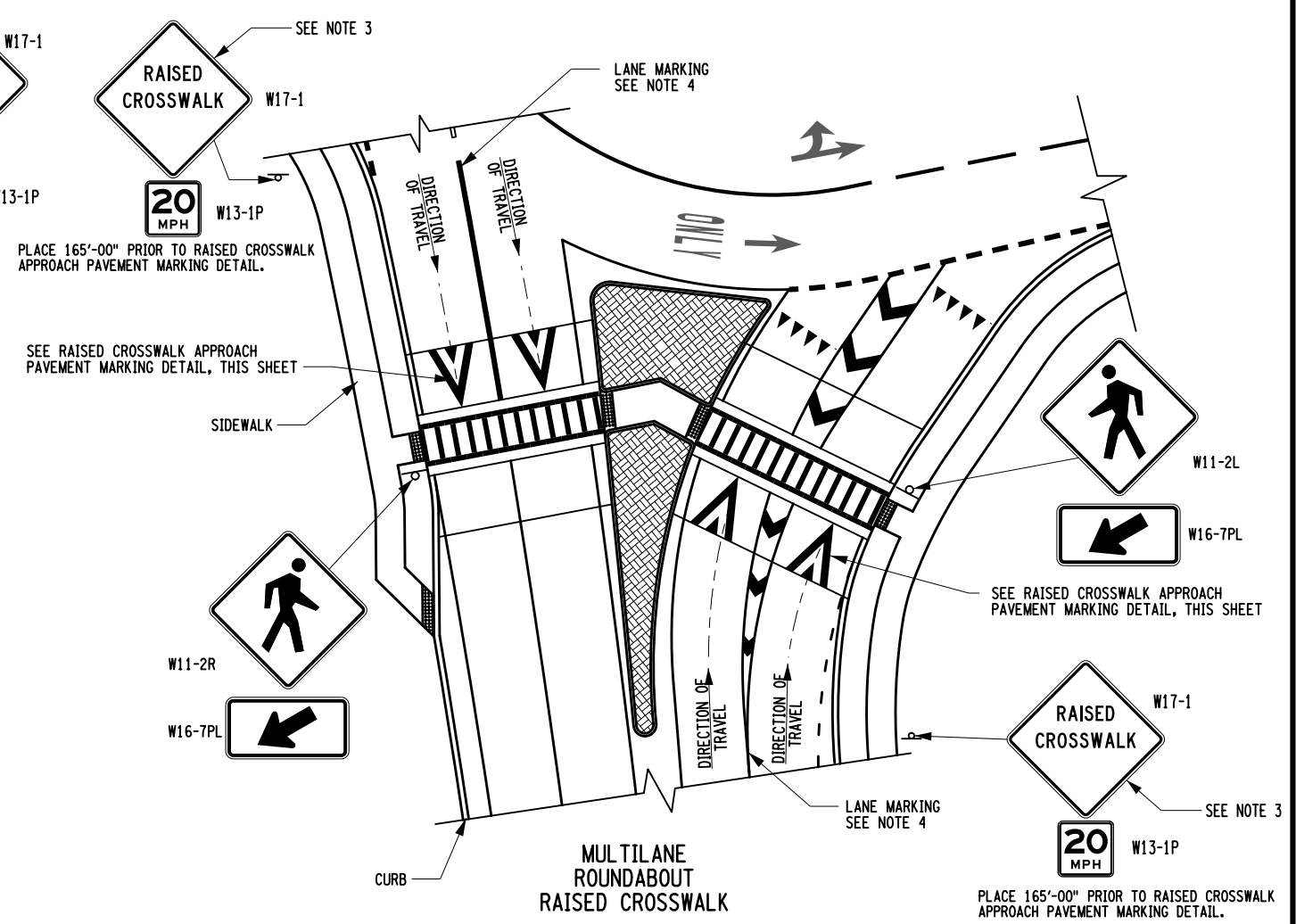
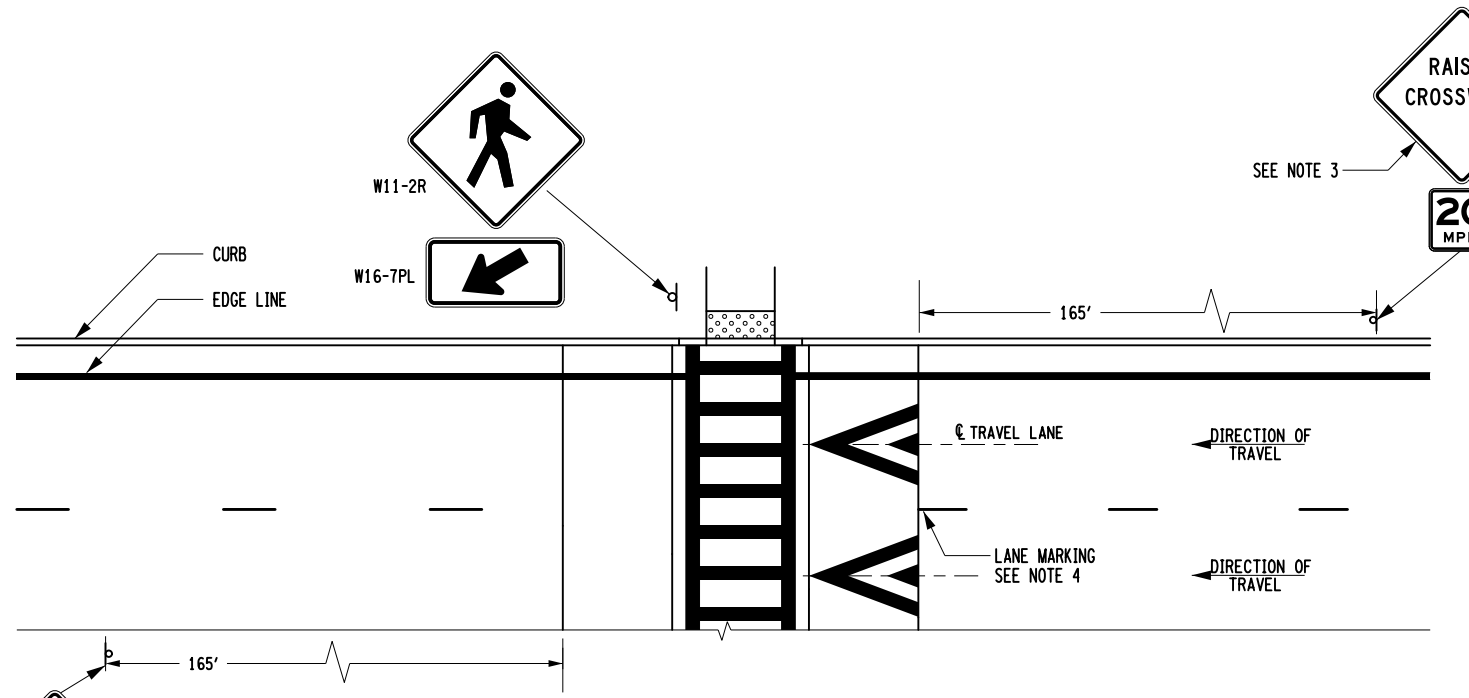
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|   |  | <b>Department of Transportation</b> |  |
| U.S. CUSTOMARY STANDARD SHEET                                 |  |                                     |  |
| <b>RAISED CROSSWALK DETAILS</b><br>(SHEET 2 OF 3)             |  |                                     |  |
| APPROVED MARCH 07, 2016                                       |  | ISSUED UNDER EB 16-012              |  |
| /S/ RICHARD W. LEE, P.E.<br>DEPUTY CHIEF ENGINEER<br>(DESIGN) |  | 608-07                              |  |



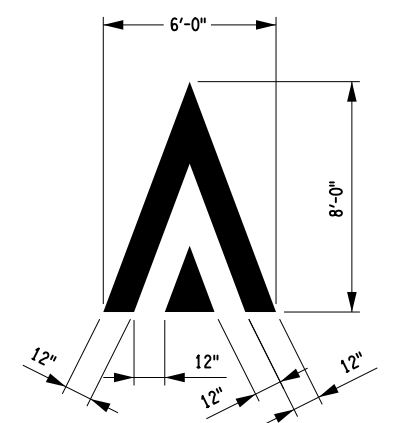
MIDBLOCK RAISED CROSSWALK  
ADVANCE PAVEMENT MARKING DETAIL  
TWO LANE



MIDBLOCK RAISED CROSSWALK  
ADVANCE PAVEMENT MARKING DETAIL  
FOUR LANE




MULTILANE  
ROUNDBOUT  
RAISED CROSSWALK



RAISED CROSSWALK APPROACH  
PAVEMENT MARKING DETAIL

NOTES:

1. FOR CONTRAST OR AESTHETICS, TYPE S CROSSWALK PAVEMENT MARKING MAY BE USED WITH A COLORED OR TEXTURED CROSSWALK SURFACE.
2. FOR ONE-WAY TRAFFIC, PAVEMENT MARKING SHALL BE PLACED ON APPROACH SIDE ONLY.
3. REFER TO THE MUTCD FOR ADDITIONAL ADVANCED WARNING SIGNS, TYPE AND LOCATION.
4. FOR CONCRETE RAISED CROSSWALKS, TERMINATE LANE MARKINGS AT THE BEGINNING OF THE LINEAR TRANSITION FOR CONCRETE, EXCEPT AT ROUNDABOUTS, WHERE LANE MARKINGS SHALL TERMINATE AT THE CROSSWALK MARKING. FOR HMA RAISED CROSSWALKS, LANE MARKINGS SHALL TERMINATE AT THE CROSSWALK MARKING.
5. FOR CROSSWALK MARKING DETAILS, REFER TO STANDARD SHEET 685-01, SHEET 2 OF 9.

|  |  |                                     |  |
|--|--|-------------------------------------|--|
|  <b>NEW YORK</b><br>STATE OF OPPORTUNITY. |  | <b>Department of Transportation</b> |  |
| U.S. CUSTOMARY STANDARD SHEET  |  |                                     |  |
| <b>RAISED CROSSWALK DETAILS</b><br>(SHEET 3 OF 3)  |  |                                     |  |
| APPROVED MARCH 07, 2016  |  | ISSUED UNDER EB 16-012              |  |
| /S/ RICHARD W. LEE, P.E.<br>DEPUTY CHIEF ENGINEER<br>(DESIGN)  |  | 608-07                              |  |

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